



## Embracing a cycling transformation

*Photo: A mother riding on a cycle with her 2 children on a busy road in Chennai*  
Source: ITDP India

# Why cycle now?

**Need for physical distancing** - Cycling is a single-person mode that ensures physical distancing and safe commute.

**Reduced public transport capacity** - Cycling is an alternative for short and medium distance trips and can support public transport through last-mile connectivity.

**Lack of affordable transport options** - Cycling provides equal travel opportunities to all sections of society and is an affordable mode of travel.



*Photo: People have started cycling to work during post the lockdown in Chennai, June, 2020*

*Source: ETimes*

## Streets that **DARE** you to cycle



Photo: A congested street with no room for cyclists  
Source: ITDP India

## Streets that **ENABLE** you to cycle



Photo: A shared cycle lane, with the risk of accidents and encroachment  
Source: The Hindu

## Streets that **INVITE** you to cycle



Photo: Segregated cycle track with shade in Pune  
Source: ITDP India

# What should be our vision?

**Cities should respond to COVID-19 by adopting cycling for a green recovery.**

**Going ahead, investing in complete streets and active mobility will yield economic benefits, build resilience towards future crises, and reduce congestion and pollution.**

# Goals to be set by cities

## Create safe infrastructure



**Protected cycle track - Sydney**  
Source: NACTO

## Ensure access to cycles for all



**People of different age groups commuting in cycle - Chennai**  
Source: ITDP India

## Break the stigma around cycling



**Car-Free Sundays initiative to promote cycling - Chennai**  
Source: ITDP India

# Goal 01: Create safe infrastructure

## Why?

- To provide equitable allocation of road space to cyclists
- Designated cycling infrastructure will make streets safer and encourage more people to take up cycling as primary mode of transport
- To strengthen the city's priority towards sustainable modes of transport and it's users



Photo: Segregated bicycle lane in Toronto, Ontario, May, 2020

Source: Bloor Annex BIA

# Intervention 1: Pop-up cycle lanes

## Where?

- Arterial, sub-arterial, and collector streets
- Along existing public transport corridors - bus, rail or metro routes
- Network should connect to landmarks - hospitals, institutions, markets, etc.

## How?

On-street parking space or even carriageway lanes can be repurposed and demarcated through paint and barricades.



Photo: Pop-up cycleways in Berlin using temporary materials, May, 2020

Source: Space for Health

## Intervention 2: Slow zones

### Where?

- Streets with constricted right of way for segregated footpaths and cycle tracks
- Areas frequented by children and elders for access to hospitals, open spaces, and schools

### How?

If carriageway is shared by cyclists and motorized transport users, speed limit should be 30kmph.

If pedestrians and motorized transport share space, speed limit should be 15kmph.

Implement road closures through barricades and timed management of traffic flow.



*Photo: Chicago's "Open Streets" program will transform residential streets for recreation May, 2020*

*Source: John Greenfield*



## Intervention 3: On-street & off-street parking

### Where?

- In markets, transit points, job centers, healthcare centres, institutions, and all city-level public spaces.

### How?

On-street and Off-street car parking slots can be reclaimed to create dedicated parking zone for cycling.

Cycle parking should be provided at the entrance to the buildings and close to public spaces to enable easy access.



*Photo: Four car parking spots reclaimed to make space for 40 bicycles in Gurugram, May, 2017*

*Source: WRI India*

# Intervention 4: Temporary shading elements

## Where?

- Along street corridors that are repurposed with pop-up cycle lanes
- At the docking stations of Public Bicycle Sharing systems

## How?

Low-cost sheets, fabric, and local materials can be creatively used to form shading elements

If continuously shaded corridors cannot be created, installations can be put up at a regular interval of 250 m - 1 km as resting spaces for cyclists.



Photo: Creating temporary shade for pedestrians/cyclists with fabric in Coimbatore, Dec, 2019

Source: Urban Design Collective

# Intervention 5: Bicycle box at intersections

## Where?

- At every signalled intersection in arterial, sub-arterial, and collector streets

## How?

Mark 5m deep space from the stop line as bicycle box, to enable two rows of waiting for cyclists

The pop-up cycle lanes should have continuous marking across intersections, with delineated bicycle boxes.



*Photo: Bike boxes reduce risk of accidents and increase visibility of cyclists, Nov, 2016*

*Source: YIMBY*

## Long-term recommendations

Create NMT cell &  
city-wide bicycle plan



**Cycling network plan, Erode**  
Source: ITDP India

Implement traffic  
demand management  
measures



**Congestion pricing in London**  
Source: Institute of  
Transportation Studies

Include shower  
facilities &  
off-street parking



**Parking lot for cycles in Japan**  
Source: gaadi.com

Increase green cover  
for shade

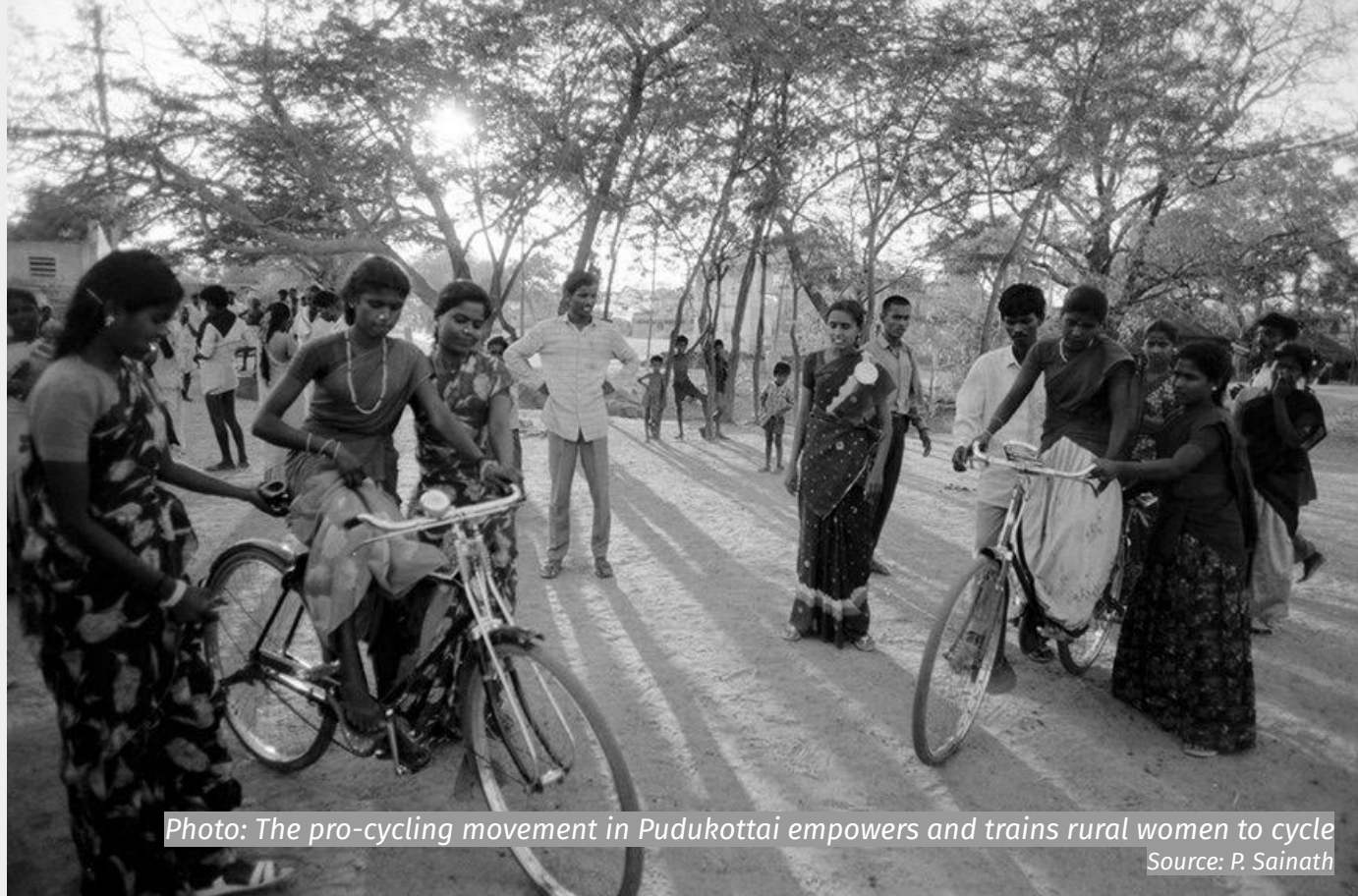


**Well shaded street with wide  
footpath in Chennai**  
Source: ITDP India

## Goal 02: Ensure access to cycles for all

### Why?

- To empower women, students and other vulnerable users in cities to move around independently
- To increase ownership of cycles
- To promote cycling as the preferred mode for transport.



*Photo: The pro-cycling movement in Pudukottai empowers and trains rural women to cycle*

*Source: P. Sainath*

# Intervention 1: Bicycle schemes

## How?

- Schemes for low-income groups, women, and students to own cycles
- Subsidies for frontline workers to own cycles



*Photo: Distribution of free cycle for school students in Chennai, Feb, 2020*

*Source: Govt. press release*

# Intervention 2: Bicycle-sharing, rental and micro-mobility systems

## How?

- Start operations of existing Public Bicycle Sharing (PBS), cycle rentals and micro-mobility systems.
- Explore affordable 'e-mobility' options through regulated norms and guidance
- Expand stations outside healthcare centers, transit stations, institutions, and government offices.
- Expand the network in alignment with the pop-up cycle lanes



Photo: Public bike sharing station in Ranchi, Oct 2019

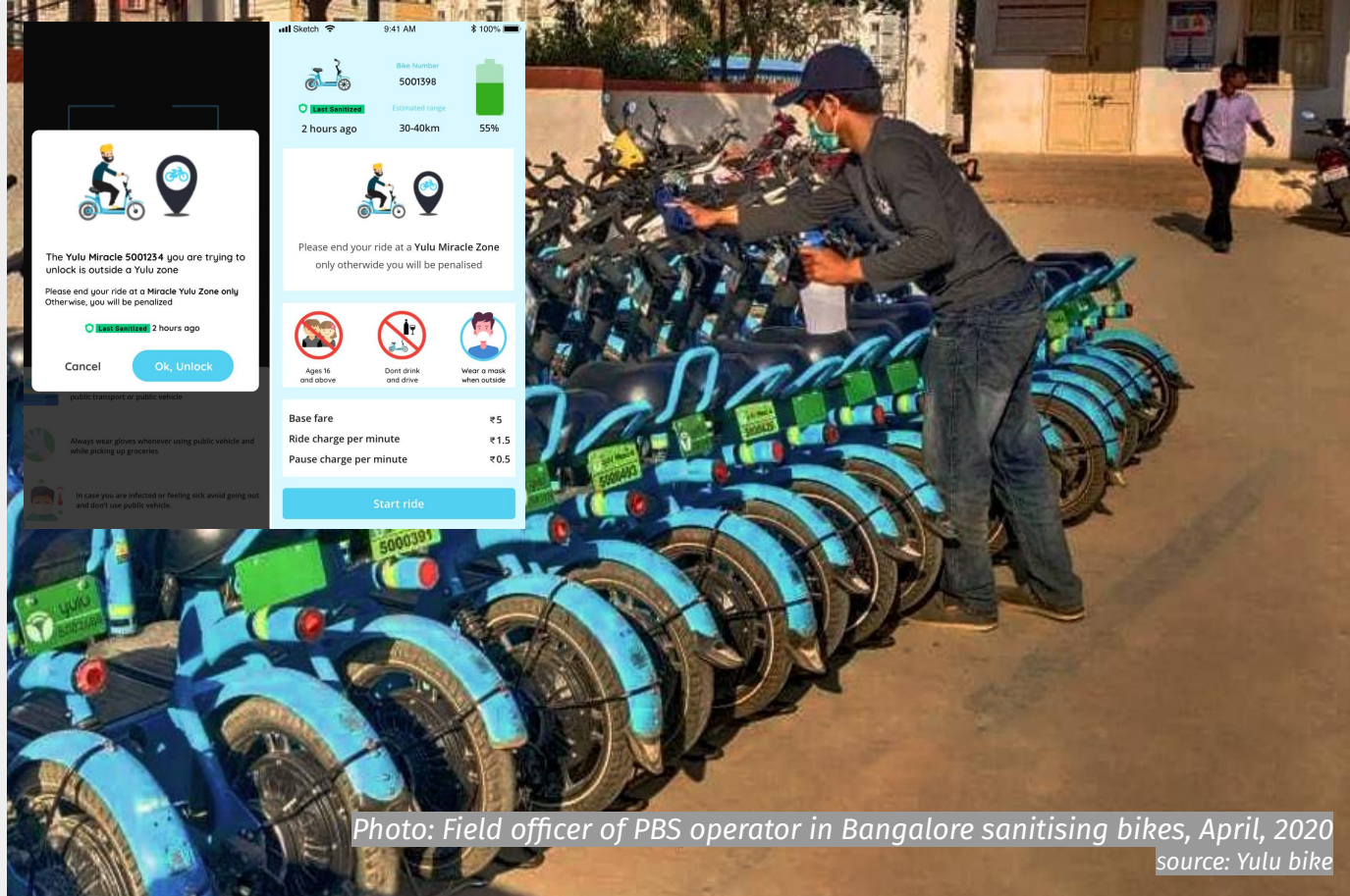
Source: ITDP India

# Intervention 2a: Regular sanitisation procedures

## Promote Public Bicycle Sharing (PBS), rentals, and micro-mobility

### How?

- Cycles must be sanitised after each trip or at regular intervals during the day
- Cycles can be sanitised on site or at the depot
- Details about the last sanitisation should be made available to the user
- Cyclists should use personal helmets and sanitise their hands before each trip





# Intervention 2b: Ensuring inclusivity

## Promote Public Bicycle Sharing (PBS), rentals, and micro-mobility

### How?

- Provide information plaques and apps in local languages.
- Integrate usage with common mobility card/smart card system rather than app-based interfaces.
- Identify cycles as mobility aid for the disabled and allow them to use cycleways



Photo (L to R): Map of the cycling network in Austria, mobility card for PBS will make it more user-friendly  
Source (L to R): Mobility and Transport, Gaadi.com

### Promote Public Bicycle Sharing (PBS), rentals, and micro-mobility

#### How?

- Introduce monthly and weekly passes
- Offices can initiate cycle-to-work schemes to offer subsidies for employees



Photo: Kochi metro when launching PBS, offered 100 hours of free cycling every month  
source: The News Minute

# Intervention 3: Subsidise cycle maintenance

## How?

- Bike shops to be declared as essential businesses
- Cities can offer cyclists an allowance for repair and maintenance
- Mobile cycle vans can offer easy access to cycle repairs for all citizens



Photo: A community supported mobile bike repair van in Colorado, May, 2017

Source: Mobile Bikeman

## Long-term recommendations

**Scale-up PBS systems,  
micro-mobility for all  
users**



**Management plan to  
increase e-bike fleet**  
Source: Yulu bike

**Facilitate  
state-sponsored  
community cycle sharing**



**Cyclothon conducted in  
Bangalore**  
Source: trintrin

**Offer training  
programs for women  
and students**



**Cycle training program  
conducted in a village in TN**  
Source: The Wire

**Promote 'cycling to  
work'**



**Bike to work initiative in  
Vancouver**  
Source: Mobike

# Goal 03: Break the stigma around cycling

- Promote cycling for all through campaigns and events
- Create local champions and involve communities
- Devise surveys, polls, and interactive applications

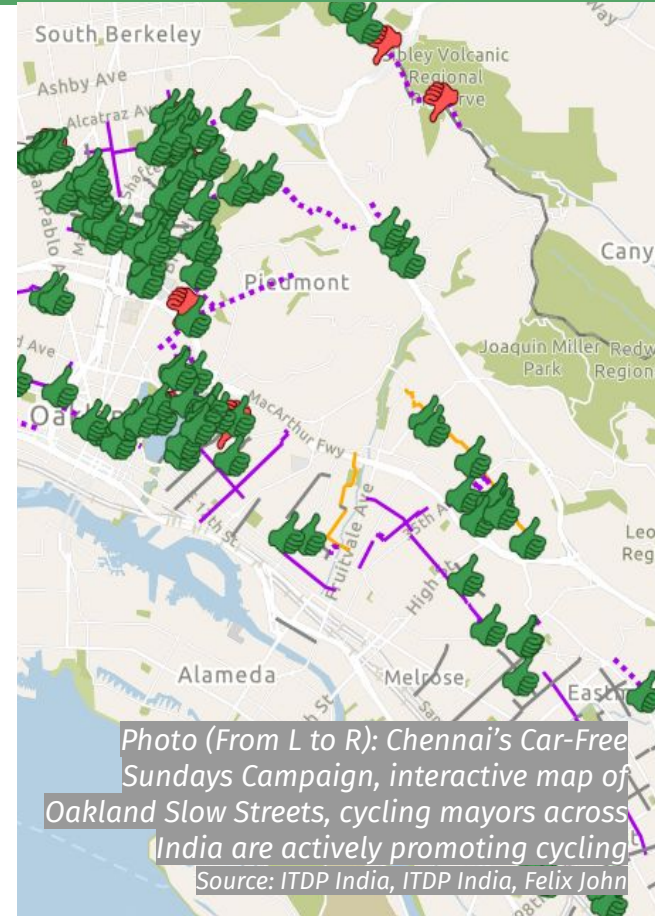
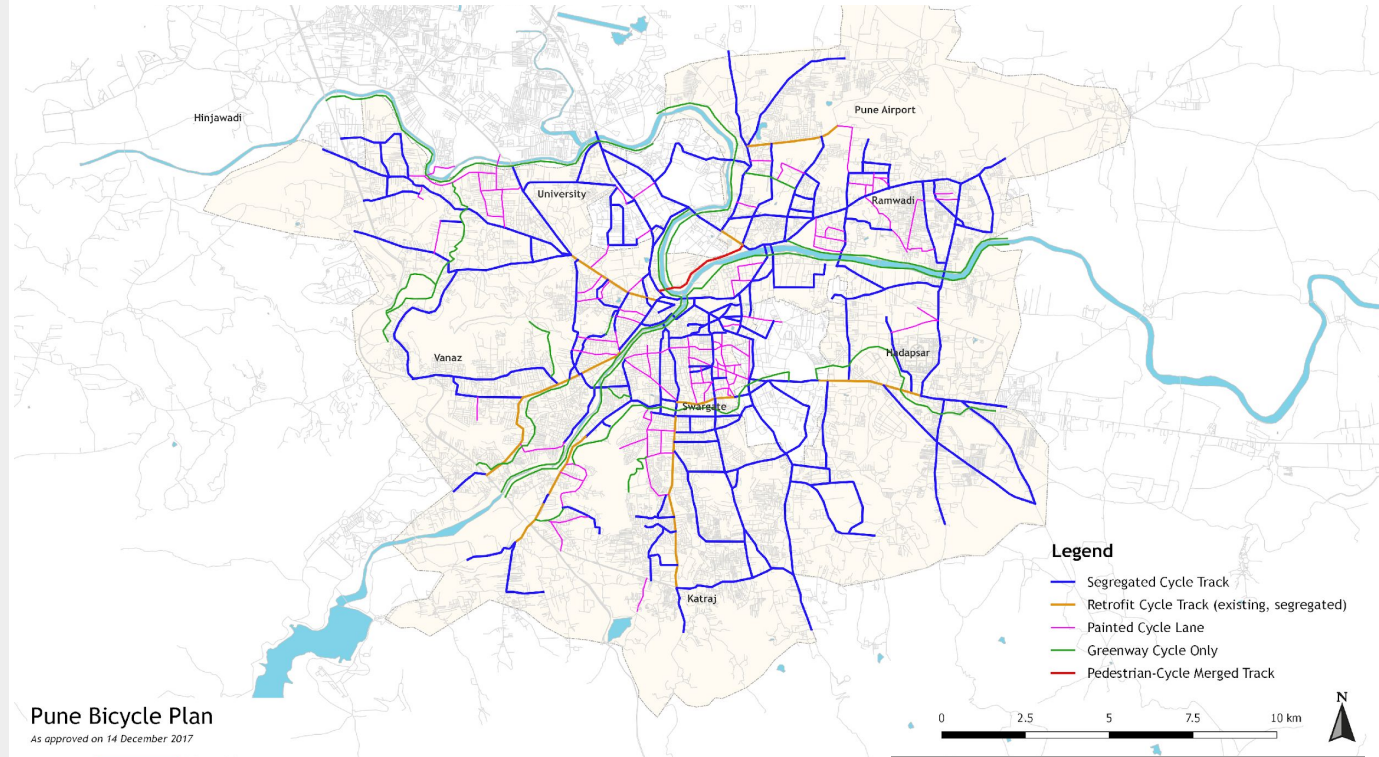


Photo (From L to R): Chennai's Car-Free Sundays Campaign, interactive map of Oakland Slow Streets, cycling mayors across India are actively promoting cycling  
 Source: ITDP India, ITDP India, Felix John

# Long-term investments in cycling

- Develop comprehensive investment strategies
- Create employment opportunities in the cycling industry
- Provide tax incentives on bicycles and its spare parts
- Encourage research and training in cycling



Pune Bicycle Plan

As approved on 14 December 2017

Photo: Pune's Bicycle Plan, Dec, 2017

Source: ITDP India

## Cultural change to make healthy streets

A once-in-a-lifetime chance to take a fresh look at our streets and make it possible for everyone to get around safely.

Cities should use the pandemic as a way to build resilience through sustainable mobility.



Photo: Congested roads of Delhi  
Source: Blogspot



Photo: Cycling movement at Amsterdam in 1982  
Source: Wikipedia

# Thank you!

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